

§ 2. FIGURE 1: CROSS-SECTION — SHOULDER SECTION.

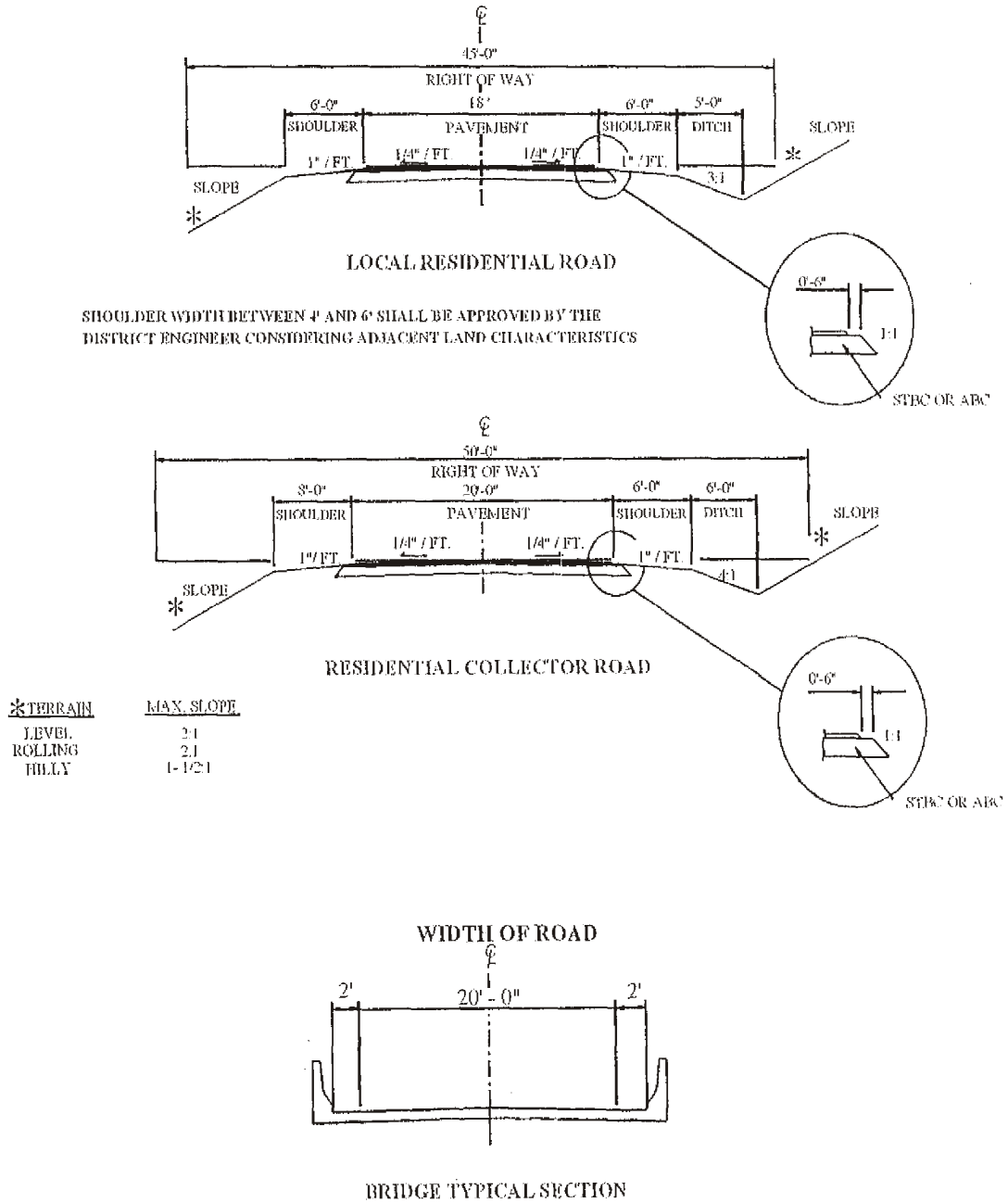
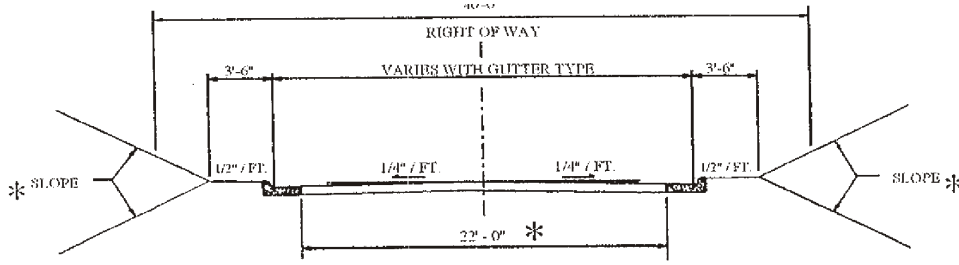


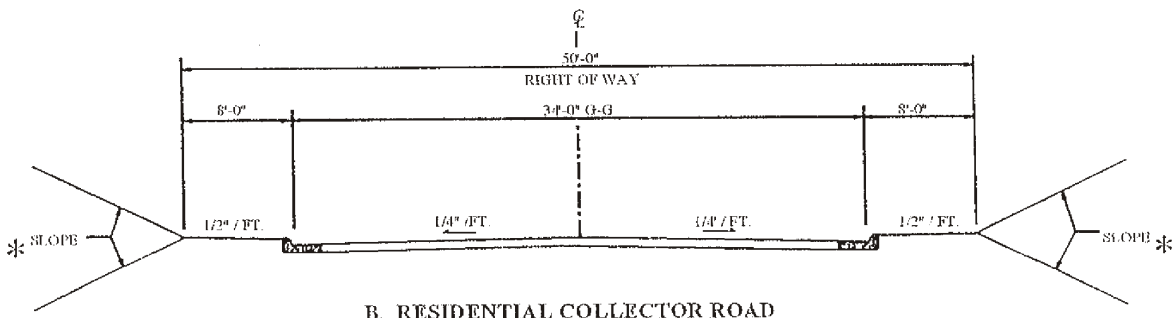
FIGURE 1: CROSS-SECTION-SHOULDER SECTION

§ 3. FIGURE 2: CROSS-SECTION — CURB & GUTTER SECTION.



A. LOCAL RESIDENTIAL ROAD

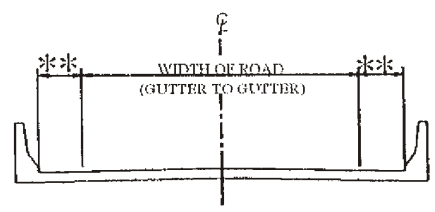
* NOTE:
ON A MAXIMUM OF TWO-TENTHS
OF A MILE IN LENGTH, 18'
OF PAVEMENT MAY BE USED



B. RESIDENTIAL COLLECTOR ROAD

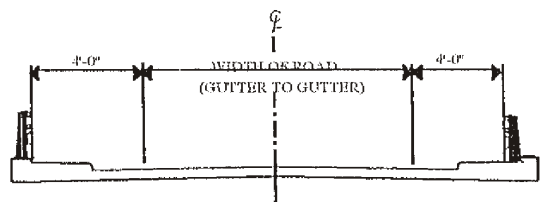
* TERRAIN	MAX SLOPE
LEVL.	2:1
ROLLING	2:1
HILLY	1-1/2:1

NOTE:
FOR BOTH 'A' & 'B' REQUEST FOR TYPICAL SECTIONS
APPROVALS BY THE DEPARTMENT OF TRANSPORTATION
THAT INCLUDES SIDEWALKS OR FUTURE SIDEWALKS
WILL BE REVIEWED INDIVIDUALLY BY THE DISTRICT
ENGINEER



C. BRIDGE TYPICAL SECTION WITHOUT SIDEWALK

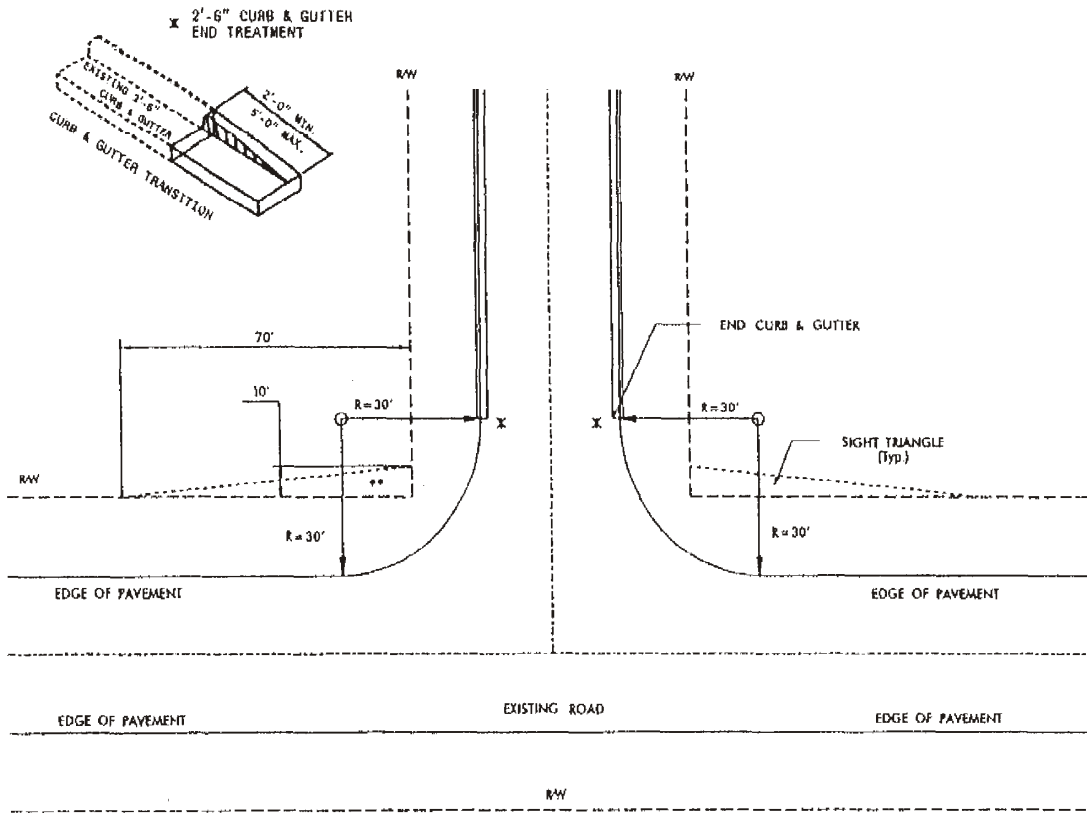
** SPEED LIMIT	OFFSET
< 45 MPH	0'
≥ 45 MPH	2'-0"



D. BRIDGE TYPICAL SECTION W/SIDEWALK

FIGURE 2: CROSS SECTION: CURB & GUTTER SECTION

§ 4. FIGURE 3: RECOMMENDED ROAD CONNECTION.



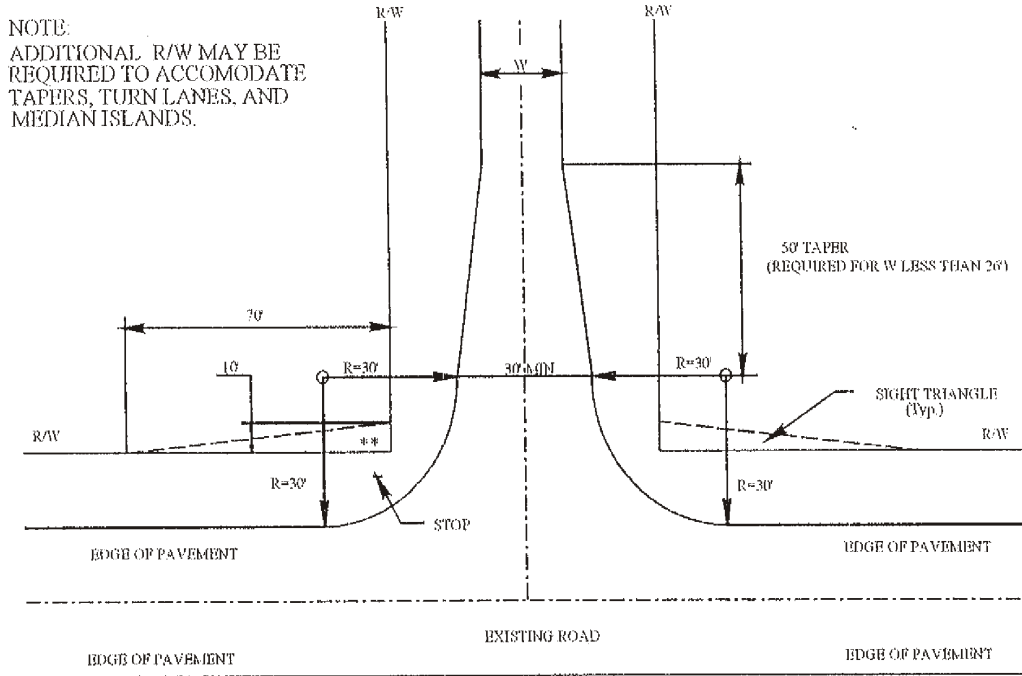
** NOTE: Permanent Drainage easements may be required to accomodate drainage beyond the right-of-way.

FIGURE 3

RECOMMENDED ROAD CONNECTION

NEW LOCAL RESIDENTIAL ROAD OR RESIDENTIAL COLLECTOR ROAD WITH CURB & GUTTER AND EXISTING STATE MAINTAINED ROAD WITH SHOULDER SECTION

§ 5. FIGURE 4: RECOMMENDED ROAD CONNECTION — STOP CONDITION.



**NOTE: Permanent Drainage easements may be required to accommodate drainage beyond the right-of-way.

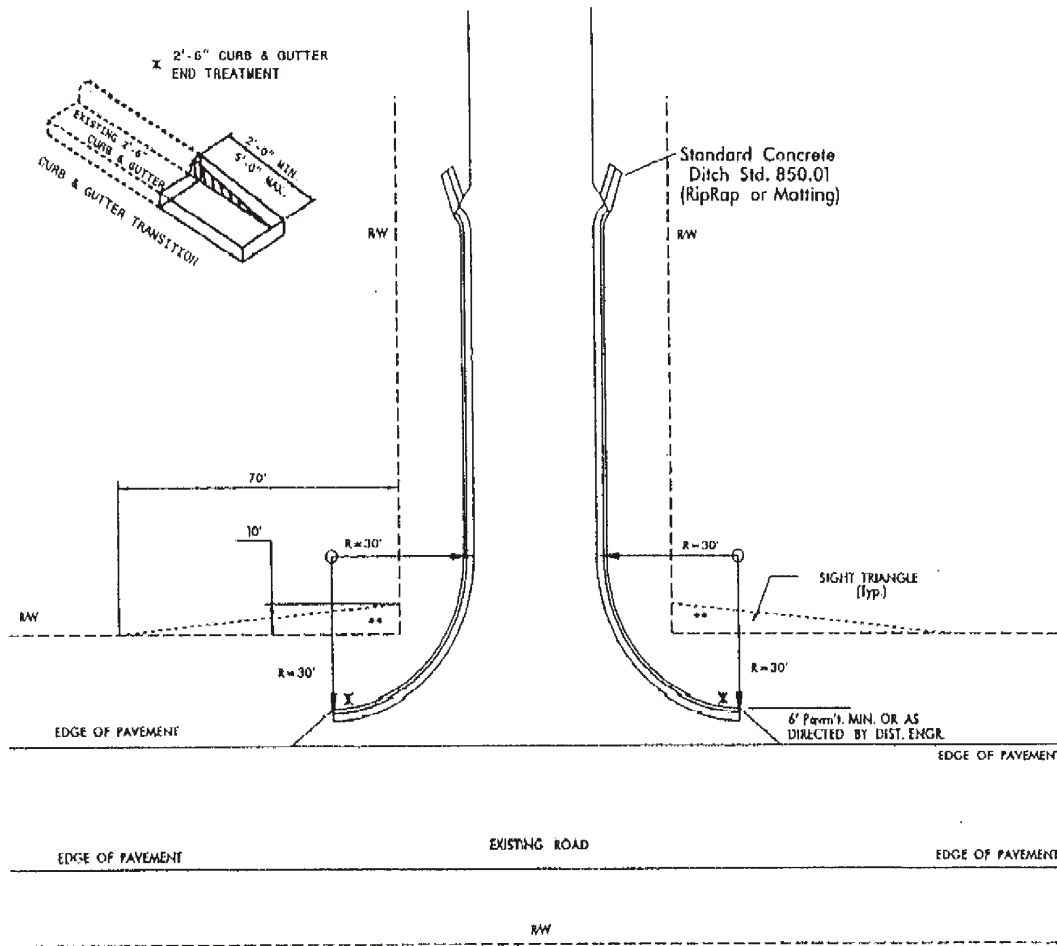
FIGURE 4

RECOMMENDED ROAD CONNECTION

NEW LOCAL RESIDENTIAL ROAD OR RESIDENTIAL COLLECTOR ROAD AND EXISTING STATE MAINTAINED ROAD WITHOUT CURB AND GUTTER

STOP CONDITION

§ 6. FIGURE 5: DRIVEWAY TURNOUT GRADES.



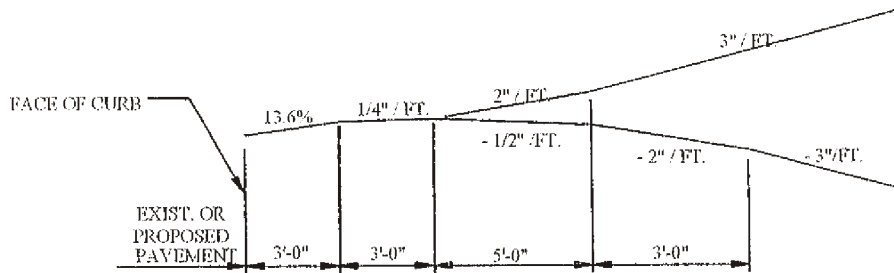
** NOTE: Permanent Drainage easements may be required to accommodate drainage beyond the right-of-way.

RECOMMENDED ROAD CONNECTION WITH CURB AND GUTTER

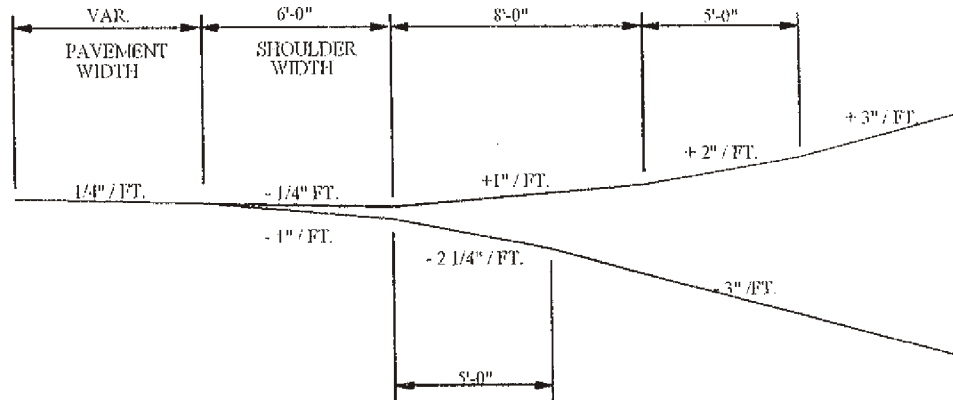
x SEE ISLAND DETAIL IF AN INTERIOR ISLAND IS TO BE INSTALLED

FIGURE 5

§ 7. FIGURE 6: SYMMETRICAL CUL-DE-SAC WITH CURB & GUTTER.



A. CURB & GUTTER SECTION



B. SHOULDER SECTION

FIGURE 6

DRIVEWAY TURNOUT GRADES

§ 8. FIGURE 7: SYMMETRICAL CUL-DE-SAC — NO CURB & GUTTER.

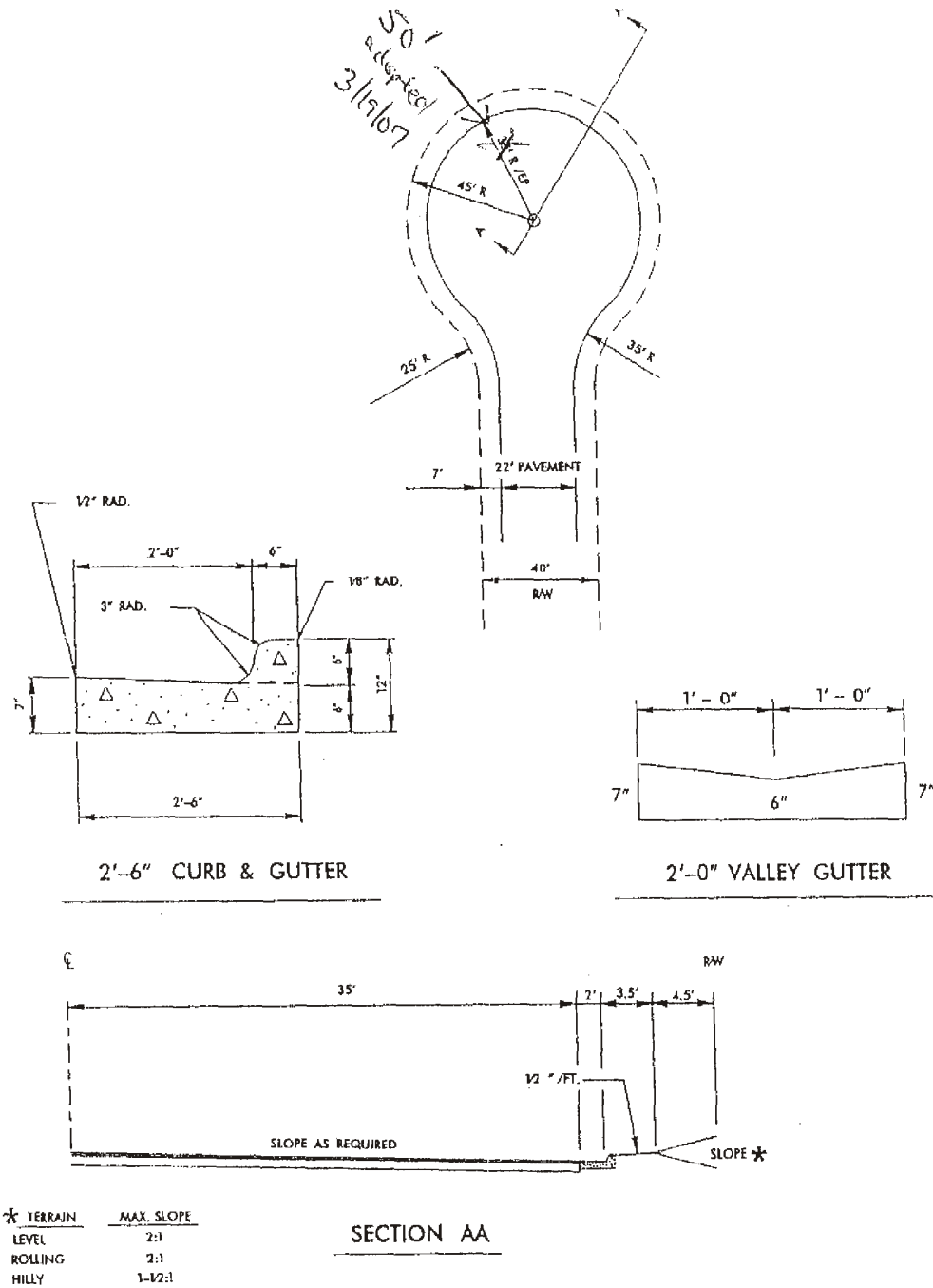
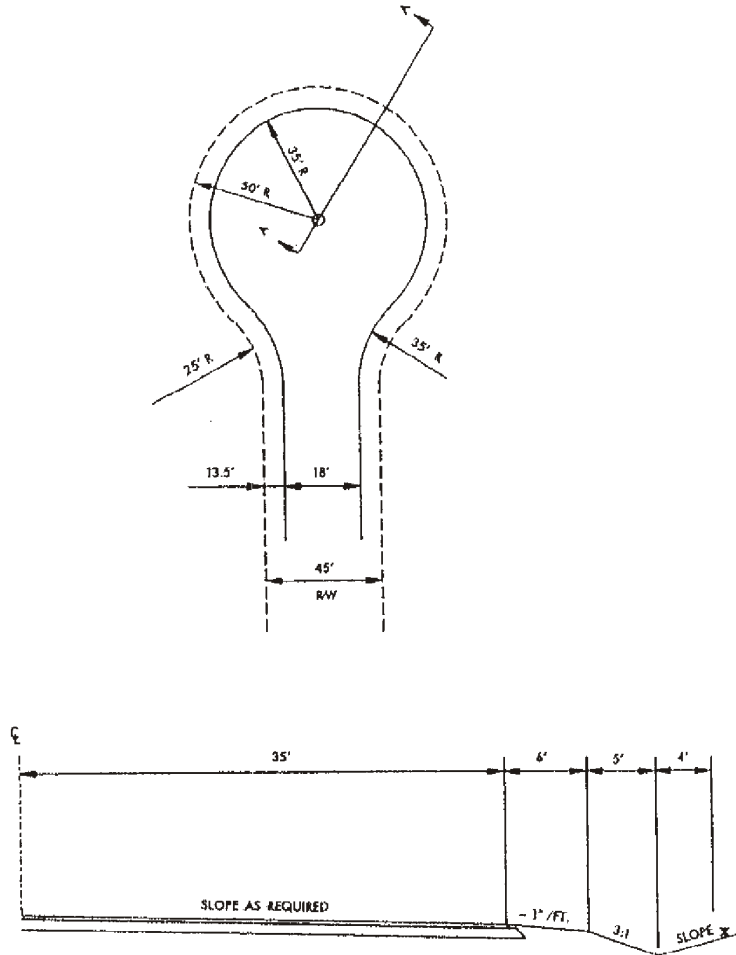


FIGURE 7
SYMMETRICAL CUL-DE-SAC WITH
CURB & GUTTER

§ 9. FIGURE 8: EXAMPLES OF MINOR RESIDENTIAL CUL-DE-SAC DESIGNS.



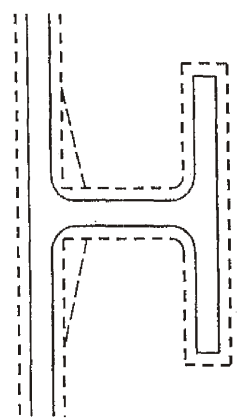
TERRAIN	MAX. SLOPE
LEVEL	2:1
ROLLING	2:1
HILLY	1-1/2:1

SECTION AA

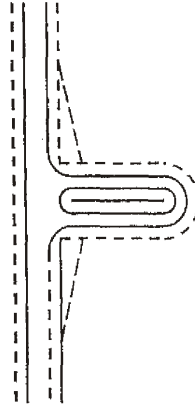
FIGURE 8

SYMMETRICAL CUL-DE-SAC

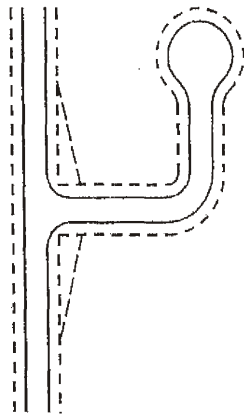
NO CURB & GUTTER



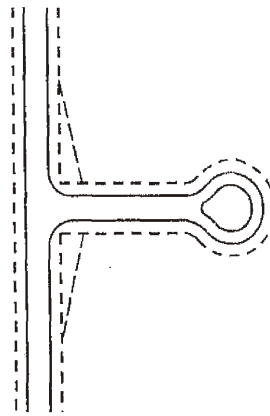
"T" CUL-DE-SAC



"LOOP" ROAD



"L" CUL-DE-SAC



CUL-DE-SAC WITH INTERIOR ISLAND

NOTE: Permanent Drainage easements may be required to accommodate drainage beyond the right-of-way. Cul-de-sac designs other than the "Bulb" End Design will be subject to the approval of the Division Engineer after review on an individual basis.

FIGURE 9
EXAMPLES OF MINOR
RESIDENTIAL CUL-DE-SAC DESIGNS

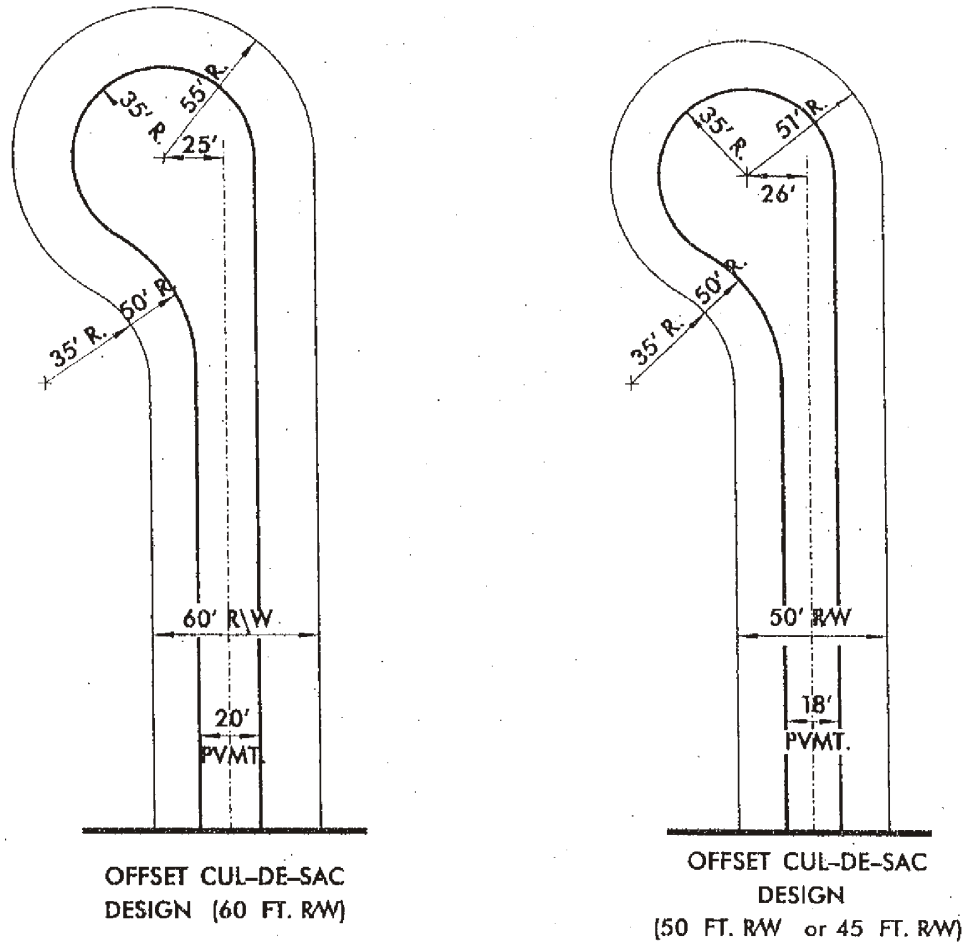
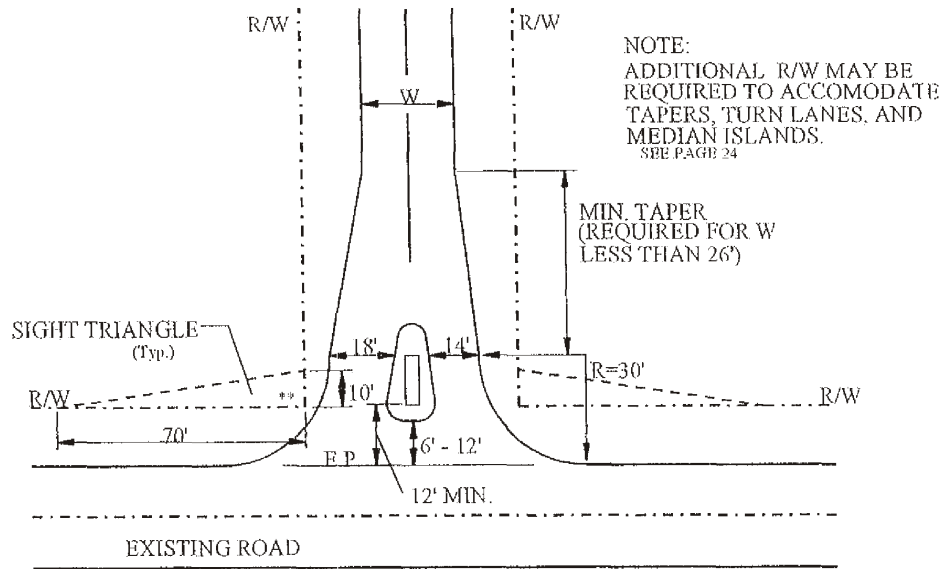


FIGURE 11
OFFSET CUL-DE-SAC DESIGN

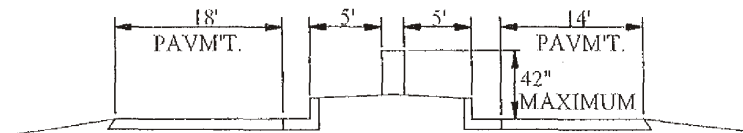
NOTE: THESE TYPICALS MAY BE LEFT OR
RIGHT AND MAY BE BUILT WITH
45' RIGHT OF WAY.

2'-0" VALLEY GUTTER



**STOP CONDITION
RECOMMENDED ROAD CONNECTION
WITH INTERIOR ISLAND**

**NOTE: Permanent Drainage easements may be required to accommodate drainage beyond the right-of-way.



**DETAIL SECTION VIEW OF INTERIOR
ISLAND AND MARKER**

FIGURE 12

